

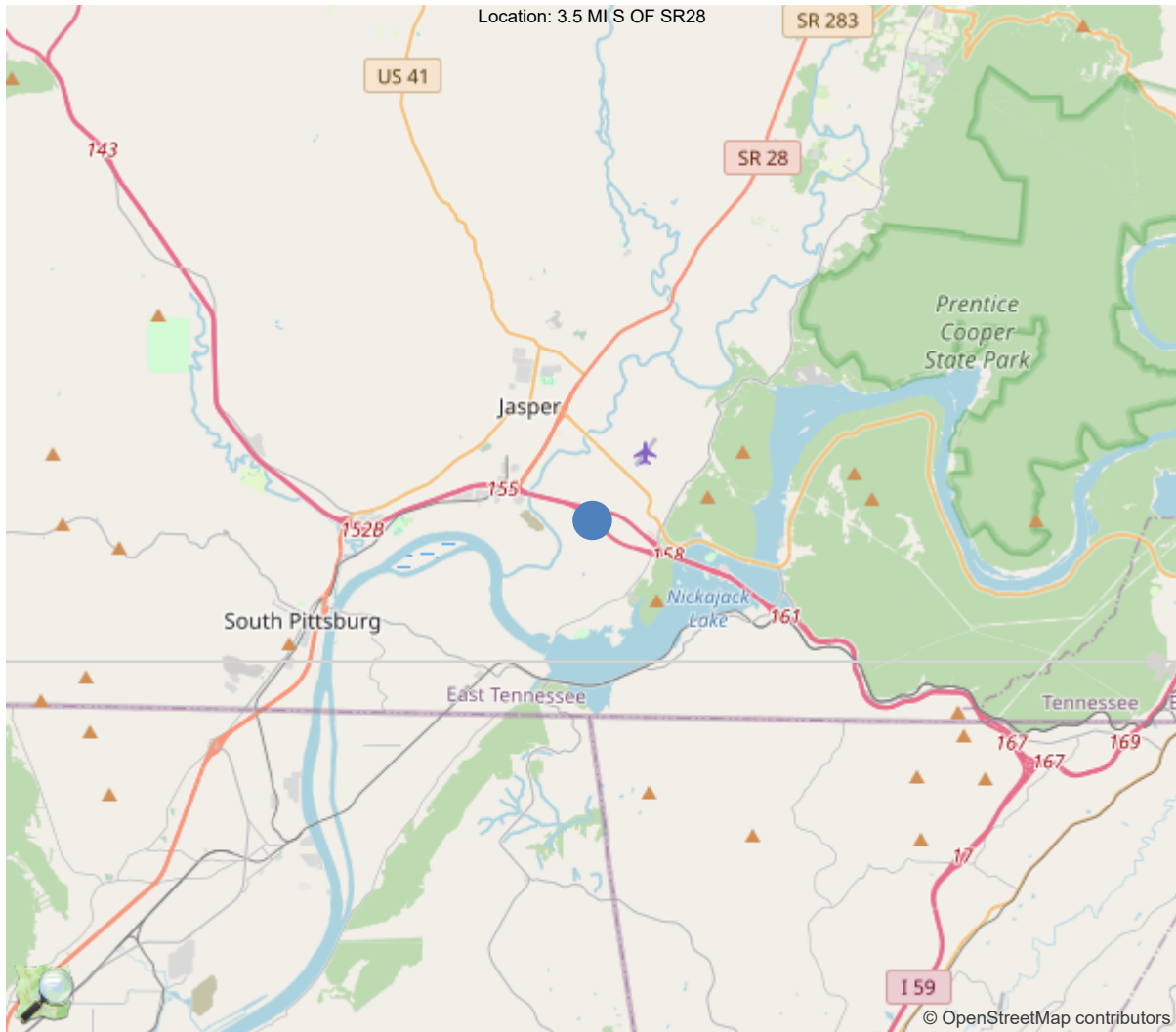


Latitude:35.04068, Longitude:-85.60353

Region 02, 58 - Marion County

Team Leader: Derek Yates

Inspectors: Anthony Pack

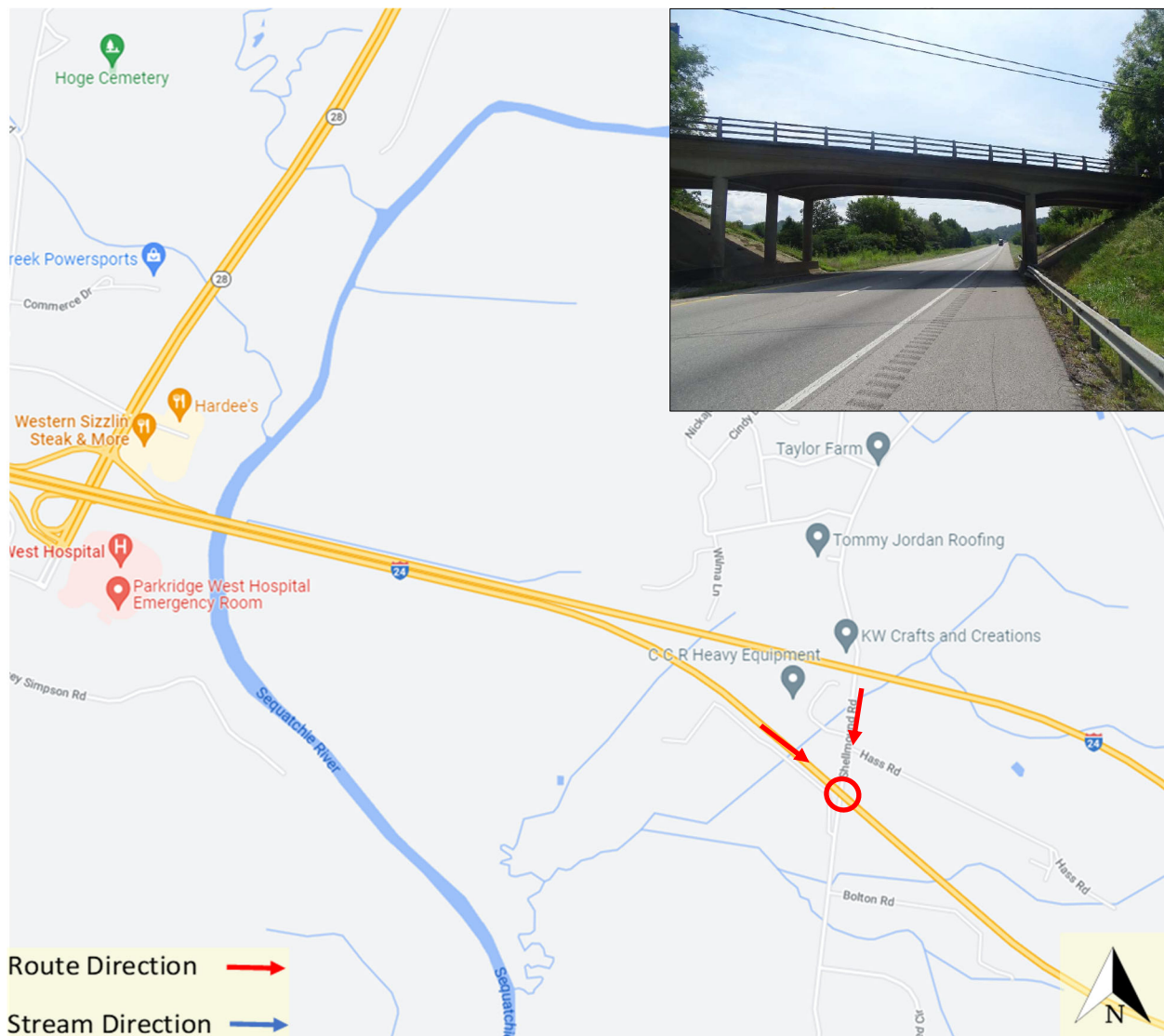


SHELLMOUND RD. Crossing SHELLMOUND RD. / I-24 EB

35.04068, -85.60353

Routine Bridge Inspection Report

Marion County	
Federal ID	58100240039
Location	58-02161-01.36
Description	Shellmound Road over Interstate 24 Eastbound Lanes, I-24 Milepost 157.29
GPS Coordinates	35.040683, -85.603533
Date	7/17/2024
Overall Condition	Fair



Maintenance Recommendations

525 - Repair List # N

523 - Repair List Add Date

524 - Repair List Revise Date 7/13/2022

Date Added	Recommendation	Priority
09/17/2002	INSTALL OBJECT MARKER SIGNS AT APPROPRIATE LOCATIONS	
09/22/2004	APPROACH GUARDRAILS ARE SUBSTANDARD	
07/25/2016	CLEAR DRAINS AT APPROACH #1 AND #2	
09/22/2004	BRIDGERAILS ARE SUBSTANDARD	
07/13/2022	REPAIR APPROACH GUARDRAIL ON RIGHT SIDE OF BOTH APPROACHES	
08/27/2018	UNDERPASS SUBSTRUCTURE PROTECTION GUARDRAILS ARE SUBSTANDARD	
07/13/2022	REMOVE DELAMINATED CONCRETE IN BOTTOM OF DECK SPAN #2	1

90 - LAST INSPECTION DATE

07/17/2024

10 - MIN. V.C. OVER DECK
(ROADWAY + SHOULDERS)

99.99 FT.

520 - MIN. V.C. OVER DECK
(EXCLUDES SHOULDERS)

99.99 FT.

36 - TRAFFIC SAFETY FEATURES

Br. Rail	Trans.	Appr. Rail	Terminal	SPEED LIM.
0	0	0	N	30

41 - STRC OPEN/CLOSED/POSTED

P

58 - DECK

6

59 - SUPERSTRUCTURE

6

60 - SUBSTRUCTURE

7

61 - CHANL/CHANL PROTECTION

N

62 - CULVERT AND RETAIN WALL

N

71 - WATERWAY ADEQUACY

N

72 - APPROACH RDWY ALIGNMENT

6

521 - OVERALL CONDITION

2 - Fair

16 - LATITUDE	17 - LONGITUDE
35.040683	-85.603533

N NOT APPLICABLE

9 EXCELLENT CONDITION

8 VERY GOOD CONDITION - NO PROBLEMS NOTED.

7 GOOD CONDITION - SOME MINOR PROBLEMS.

6 SATISFACTORY CONDITION - MINOR DETERIORATION OF STRUCTURAL ELEMENTS.

5 FAIR CONDITION - ALL PRIMARY STRUCTURAL ELEMENTS ARE SOUND BUT MAY HAVE MINOR SECTION LOSS, CRACKING, SPALLING OR SCOUR.

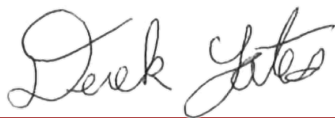
4 POOR CONDITION - ADVANCED SECTION LOSS, DETERIORATION, SPALLING OR SCOUR.

3 SERIOUS CONDITION - LOSS OF SECTION, DETERIORATION, SPALLING OR SCOUR HAVE SERIOUSLY AFFECTED PRIMARY STRUCTURAL COMPONENTS. LOCAL FAILURES ARE POSSIBLE. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT.

2 CRITICAL CONDITION - ADVANCED DETERIORATION OF PRIMARY STRUCTURAL ELEMENTS. FATIGUE CRACKS IN STEEL OR SHEAR CRACKS IN CONCRETE MAY BE PRESENT OR SCOUR MAY HAVE REMOVED SUBSTRUCTURE SUPPORT. UNLESS CLOSELY MONITORED IT MAY BE NECESSARY TO CLOSE THE BRIDGE UNTIL CORRECTIVE ACTION IS TAKEN.

1 "IMMINENT" FAILURE CONDITION - MAJOR DETERIORATION OR SECTION LOSS PRESENT IN CRITICAL STRUCTURAL COMPONENTS OR OBVIOUS VERTICAL OR HORIZONTAL MOVEMENT AFFECTING STRUCTURAL STABILITY. BRIDGE IS CLOSED TO TRAFFIC BUT CORRECTIVE ACTION MAY PUT IT BACK IN LIGHT SERVICE.

0 FAILED CONDITION - OUT OF SERVICE AND BEYOND CORREC



TEAM LEADER SIGNATURE

IDENTIFICATION	
(1) State Names	47 - Tennessee
(8) Structure Number	58I00240039
(5) Inventory Route	1
(2) Highway Agency District	Region 2
(3) County Code	58 - Marion
(4) Place Code	00000
(6) Features Intersected	SHELLMOUND RD. / I-24 EB
(7) Facility Carried	NFA 2161 (SA 5802)
(9) Location	3.5 MI S OF SR28
(11) Mile Point	1.360 mi
(12) Base Highway Network	No
(13) LRS Inventory Rte & Subrte	
(16) Latitude	35.040683
(17) Longitude	-85.603533
(98) Border Bridge State Code	
(99) Border Bridge Structure No.	
STRUCTURE TYPE AND MATERIAL	
(43) Main Structure Type	24
Material	2 - Concrete continuous
Type	4 - Tee beam
(44) Approach Structure Type	00
Material	0 - Other / None
Type	0 - Other / None
(45) No. of Spans in Main Unit	3
(46) No. of Approach Spans	0
(107) Deck Structure Type	1 - Concrete Cast-in-Place
(108) Wearing Surface/Protective System	
Type of Wearing Surface	6 - Bituminous
Type of Membrane	0 - None
Type of Deck Protection	0 - None
AGE AND SERVICE	
(27) Year Built	1965
(106) Year Reconstructed	0
(42) Type of Service	11
On	1 - Highway
Under	1 - Highway, with or without pedestrian
(28) Lane	
On	2
Under	2
(29) Average Daily Traffic	1716
(30) Year of ADT	2021
(109) Truck ADT	3 %
(19) Bypass, Detour Length	5 mi
GEOMETRIC DATA	
(48) Length of Maximum Span	60.5 ft
(49) Structure Length	139.5 ft
(50) Curb or Sidewalk Width	
Left	0.0 ft
Right	0.0 ft
(51) Bridge Roadway Width Curb to Curb	28.0 ft
(52) Deck Width Out to Out	34.5 ft
(32) Approach Roadway Width (W/Shoulders)	22.0 ft
(33) Bridge Median	0 - No median
(34) Skew	58 Deg
(35) Structure Flared	0 - No flare
(10) Inventory Route Min Vert Clear	99.99 ft
(47) Inventory Route Total Horiz Clear	28.0 ft
(53) Min Vert Clear Over Bridge Rdwy	99.99 ft
(54) Min Vert Underclear	17.00 ft
Ref:	
(55) Min Lat Underclear RT	12.0 ft
Ref:	
(56) Min Lat Underclear LT	11.0 ft
NAVIGATION DATA	
(38) Navigation Control	N - Not applicable, no waterwa
(111) Pier Protection	
(39) Navigation Vertical Clearance	0.0 ft
(116) Vert-Lift Bridge Nav Min Vert Clear	ft
(40) Navigation Horizontal Clearance	0.0 ft

CLASSIFICATION	
(112) NBIS Bridge Length	Y
(104) Highway System	0
(26) Functional Class	8 - Rural Minor Collector
(100) Defense Highway	0 - The inventory route is not
(101) Parallel Structure	N - No parallel structure exis
(102) Direction of Traffic	2 - way traffic
(103) Temporary Structure	
(105) Federal Lands Highways	0 - N/A
(110) Designated National Network	0 - The inventory route is not
(20) Toll	3 - On free road. The structu
(21) Maintain	1 - State Highway Agency
(22) Owner	1 - State Highway Agency
(37) Historical Significance	4 - Historical significance is
CONDITION	
(58) Deck	6
(59) Superstructure	6
(60) Substructure	7
(61) Channel & Channel Protection	N
(62) Culverts	N
LOAD RATING AND POSTING	
(31) Design Load	6 - MS 18+Mod / HS 20+Mod
(63) Operating Rating Method	8
(64) Operating Rating	
Type	8 - Load and Resistance Factor Rating (LRF
Rating	27.86
(65) Inventory Rating Method	8 - Load and Resistance Factor
(66) Inventory Rating	
Type	
Rating	19.44
(70) Bridge Posting	5 - Equal to or above legal loads
(41) Structure Open/Posted/Closed	P - Posted for load (may inclu
APPRAISAL	
(67) Structural Evaluation	5
(68) Deck Geometry	5
(69) Clearances, Vertical/Horizontal	3
(71) Waterway Adequacy	N
(72) Approach Roadway Alignment	6
(36A) Bridge Railings	0 - Inspected feature does not meet
(36B) Transitions	0 - Inspected feature does not meet
(36C) Approach Guardrail	0 - Inspected feature does not meet
(36D) Approach Guardrail Ends	N - Not applicable or a safety feat
(113) Scour Critical Bridges	N - Bridge not over waterway.
PROPOSED IMPROVEMENTS	
(75) Type of Work	36 - Bridge deck rehabilitatio
(76) Length of Structure Improvement	140.1 ft
(94) Bridge Improvement Cost	\$ 152
(95) Roadway Improvement Cost	\$ 16
(96) Total Project Cost	\$ 229
(97) Year of Improvement Cost Estimate	2022
(114) Future ADT	2467
(115) Year of Future ADT	2042

INSPECTIONS *			
(90) Inspection Date	07/17/2024		
(91) Frequency	24		
(92) Critical Feature Inspection	Done	Freq. (Mon)	Date
A: Fracture Critical Detail	No		
B: Underwater Inspection	No		
C: Other Special Inspection	No		
* The inspection date and frequency information in this box contains the current NBI date and frequency information. Please refer to the report header for the date this inspection was conducted.			



Looking ahead on route



Top of deck



Looking back on route



Typical bottom of deck



Right side view of structure



Left side view of structure



Underclearance looking ahead on underpass route



Underclearance looking back on underpass route



Bridge number and underpass number



Weight posting for Approach #1



Weight posting for approach #2



Advanced weight posting sign for approach #2



Typical abutment



Typical bent



Typical delaminated areas in overhang



Typical spall with exposed rebar



Typical spall on bents



Debris on top of deck



Damaged guardrail at approach #2 right

PERFORMANCE EVALUATION

Time of Day Inspected 9:20 A.M.

Weather Conditions Overcast, 89°F

Vehicles Observed Autos

LIVE LOAD BEHAVIOR

Sub Horiz./ Vert. Defl (No)

Sub Vibration (No)

Super Horiz./ Vert. Defl (No)

Super Vibration (No)

APPROACH

Alignment (Fair) Moderate vertical curves at both approaches

Slab (NA)

Joints (NA)

Pavement (Good)

Embankment (Good)

Approach Drains (Fair) Partially blocked with debris

TRAFFIC SAFETY FEATURES

Bridge Railing Rating (Good)

Transitions Rating (NA) Approach guardrail not attached to bridge railing

Guardrail Rating (Poor) Collision damage; set too low

Guardrail Terminal Rating (Poor) Collision damage; set too low

SIGNS POSTED ON ROUTE

Paddleboards	Needed	Weight Limit Posted	Yes
Vertical Clearance (<14'-6")	No	Gross	40 Tons
Posted Height		Single-unit Vehicle	Tons
Narrow Bridge Signs	No	Multi-unit Vehicle	Tons
One Lane Bridge Signs	No	564 Assigned Bridge Name	
Other Signs or Plaques			

ATTACHED SIGNS

Sign No	Location	Text on Sign	Noted Defects
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DECK

Wearing Surface Type		Asphalt	Wearing Surface Depth	4
Wearing Surface	(Fair)	Heavy vegetation at curblines; some moderate cracking		
Deck - Structural Condition	(Fair)	Some minor cracking, delaminated areas, and repaired areas; isolated minor and moderate spalls		
Curbs	(Fair)	Widespread minor transverse cracks, dirt and vegetation in curbs		
Median	(NA)			
Sidewalks	(NA)			
Parapet	(NA)			
Railing	(Good)	Substandard metal railing		
Rail Paint	(NA)			
Deck Drains	(NA)			
Lighting Standards	(NA)			
Utilities	(NA)			
Expansion Joints	(NA)	Slight to moderate leakage on abutments		

SUPERSTRUCTURE

Bearing Devices	(Good)	
Girders	(Fair)	Widespread minor cracks
Beams	(NA)	
Floor Beams	(NA)	
Stringers	(NA)	
Diaphragms	(Good)	
Superstructure Bracing	(NA)	
Trusses - General	(NA)	
Trusses - Portals	(NA)	
Trusses - Bracing	(NA)	
Superstructure Paint	(NA)	
Alignment of Members	(Good)	

TEXTURE COAT

ABUTMENTS

Abutment Caps	(Good)	
Abutment Breastwall	(NA)	
Abutment Wings	(Good)	
Abutment Backwall	(Good)	
Abutment Plumb	(Good)	
Abutment Footing	(Not visible)	
Abutment Piles	(Not Visible)	
Abutment Embankment	(Good)	
Abutment Bearing Surface	(Good)	
Abutment Slope Paving	(Good)	
Abutment Rip Rap	(NA)	

PIERS

Pier Caps	(NA)	
Pier Columns I Walls	(NA)	
Pier Plumb	(NA)	
Pier Footing	(NA)	
Pier Piles	(NA)	
Pier Bearing Surface	(NA)	

BENTS

Bent Caps	(NA)	
Bent Columns	(Good)	Some minor rebar pop-outs and delaminated areas
Bent Plumb	(Good)	
Bent Footing	(Not Visible)	
Bent Piles	(Not Visible)	
Bent Bearing Surface	(Good)	
Piles Need Replacement	(No)	

Inspection Team's Summary

This bridge is a three span concrete deck girder structure crossing I-24 eastbound lanes. It has a total length of 139.5 feet with a maximum span length of 60.5 feet and is placed on a 58° left skew. It was constructed in 1965. The bridge was inspected on July 17, 2024, by a Region 2 bridge inspection team from Tullahoma and was found to be in overall fair condition.

The approach alignment is rated fair due to moderate vertical curves at both approaches. The approach pavement is rated good. The drains are rated fair due to being partially blocked with debris. The substandard bridge railing is rated good. The approach guardrail is not attached to the bridge railing. The guardrails and terminals are substandard and rated poor. The guardrail has collision damage and is set too low. Object marker signs are needed. The approach #1 right object marker sign is defaced, and the approach #2 paddleboards are missing.

The deck is rated fair. The top of the concrete deck is not visible due to the asphalt wearing surface. There is heavy vegetation at the curblines and cracking. The bottom of the concrete deck some minor cracking, delaminated areas, repaired areas, and isolated minor and moderate spalls. The curbs are rated fair due to widespread minor transverse cracks, dirt, and vegetation in the curblines. The substandard deck railing is rated good. There is slight to moderate joint leakage on the abutments.

The superstructure is rated fair. The beams have widespread minor cracking.

The substructure is rated good. The columns are have some minor rebar pop-outs and delaminated areas.

The underpass is rated good. There are delaminated areas in overhangs in span #2 which could fall onto the roadway beneath.

General Inspection Comment

HQ notes to TL

Deck Elevation

Benchmark height 646.93 Benchmark location Top of bridge seat on right side of abutment 1. Edge location _____

Comment

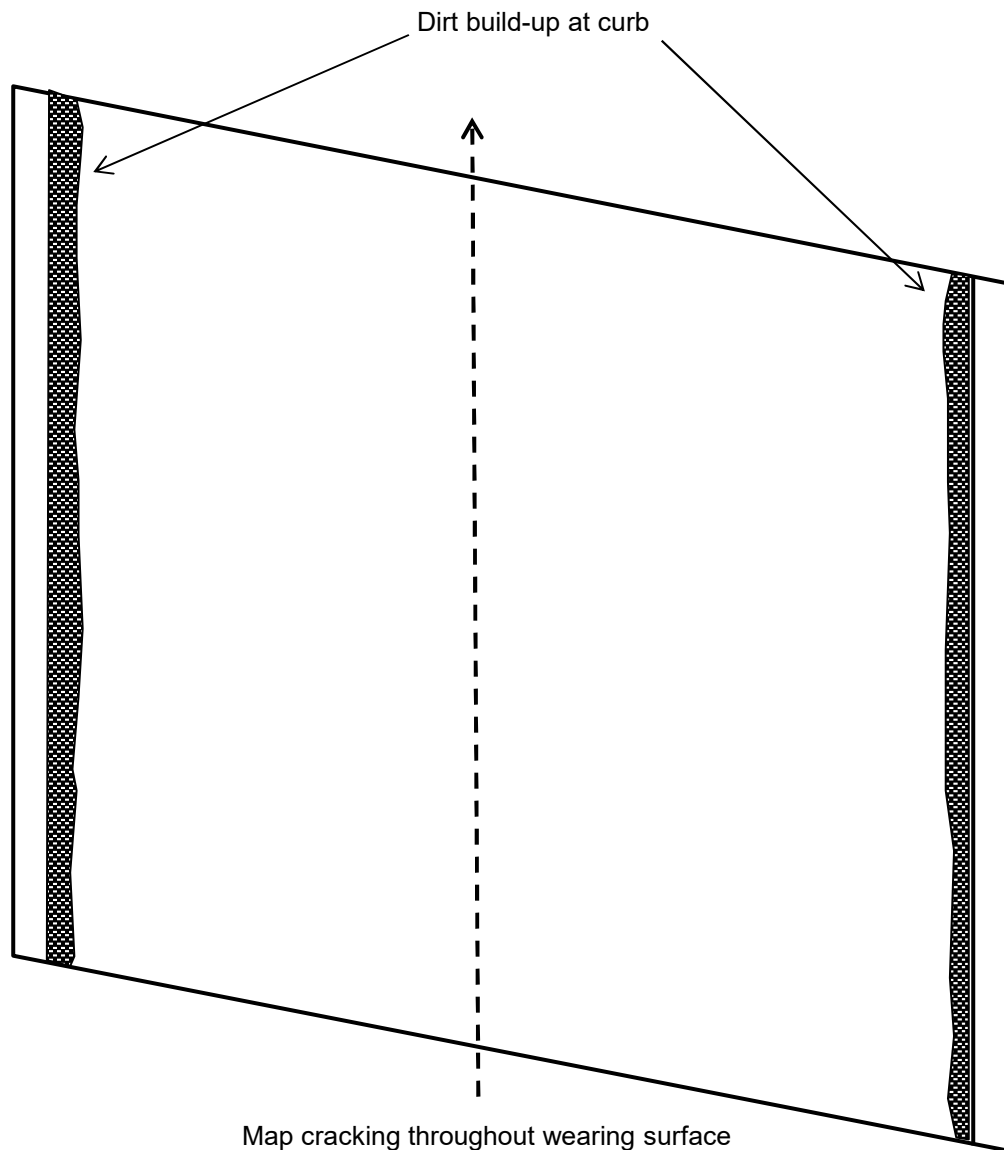
Location	Top Lt. Curb	Left Gutter	Center Line	Right Gutter	Top Rt. Curb
ABUTMENT 1	650.02	649.52		649.35	649.85
BENT 1	650.05	649.65		649.63	650.06
BENT 2	649.35	648.92		649.25	649.65
ABUTMENT 2	650.06	648.25		648.68	649.14

Top of Deck Span No. 1

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking		Repairs		(light vertical)
	Spalling		Voids		(large checker board)
	Scaling		Dimensions are noted as		
	Delamination				
		(confetti)			
		(10% dots)			
		(40% gray)			
			← X ↑ X depth		

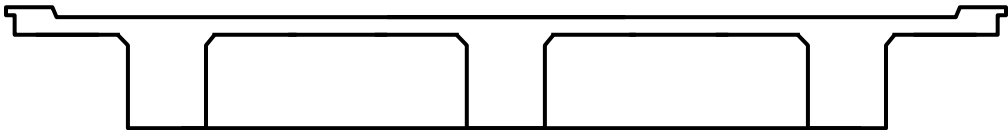


Bottom of Deck Span No. 1

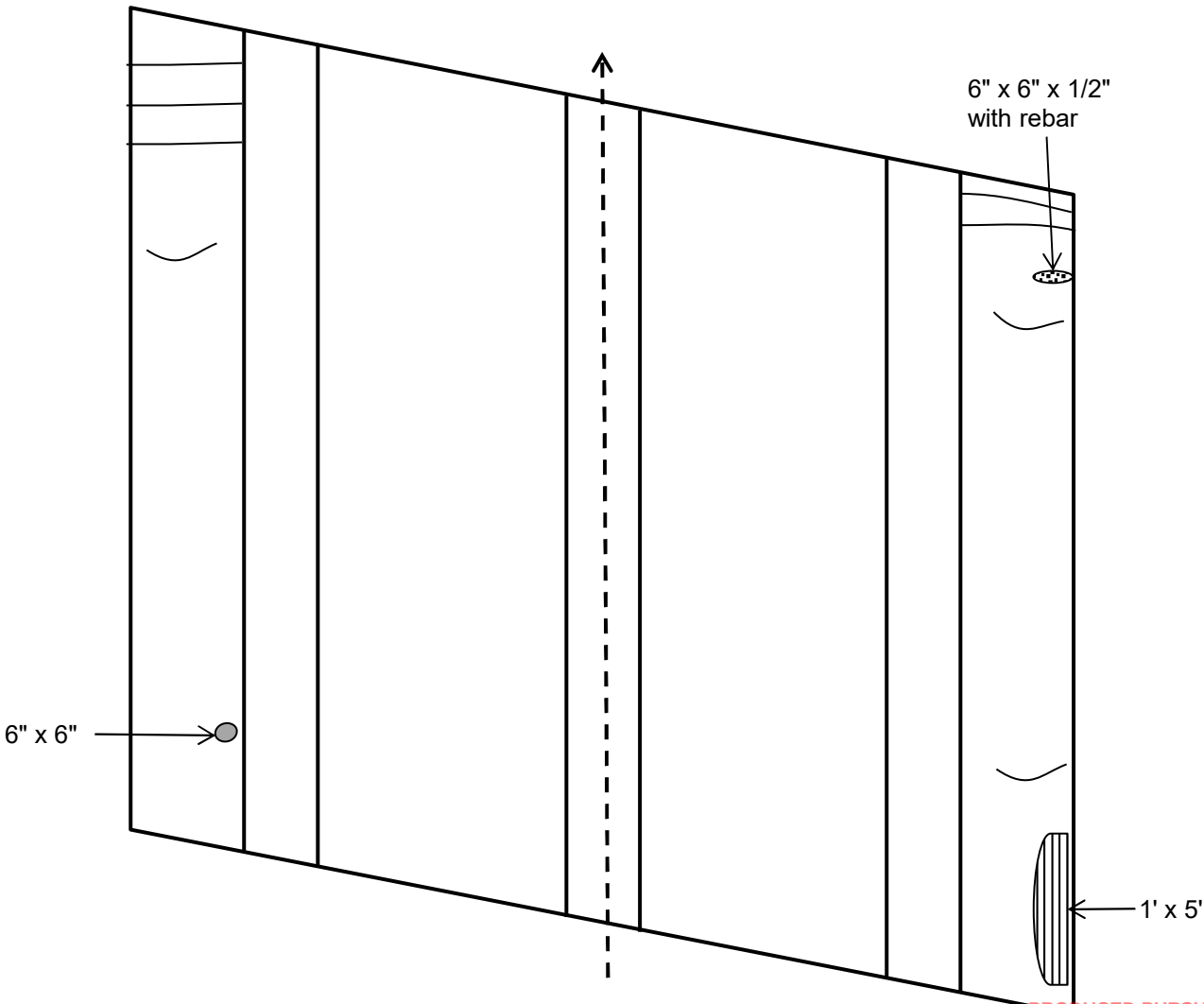
Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Voids		(large checker board)
	Scaling		(10% dots)	Dimensions are noted as		
	Delamination		(40% gray)			X depth



-Cracks are hairline with efflorescence



Beam Details - Span No. 1

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking		Repairs		(light vertical)
	Spalling		Voids		(large checker board)
	Scaling		Dimensions are noted as		
	Delamination		X X depth		

Beam "A" - Right Side



Beam "A" - Left Side



Beam "B" - Right Side



Beam "B" - Left Side



Beam Details - Span No. 1

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking		Repairs		(light vertical)
	Spalling		Voids		(large checker board)
	Scaling		Dimensions are noted as		
	Delamination		X X depth		

Beam "C" - Right Side



Beam "C" - Left Side

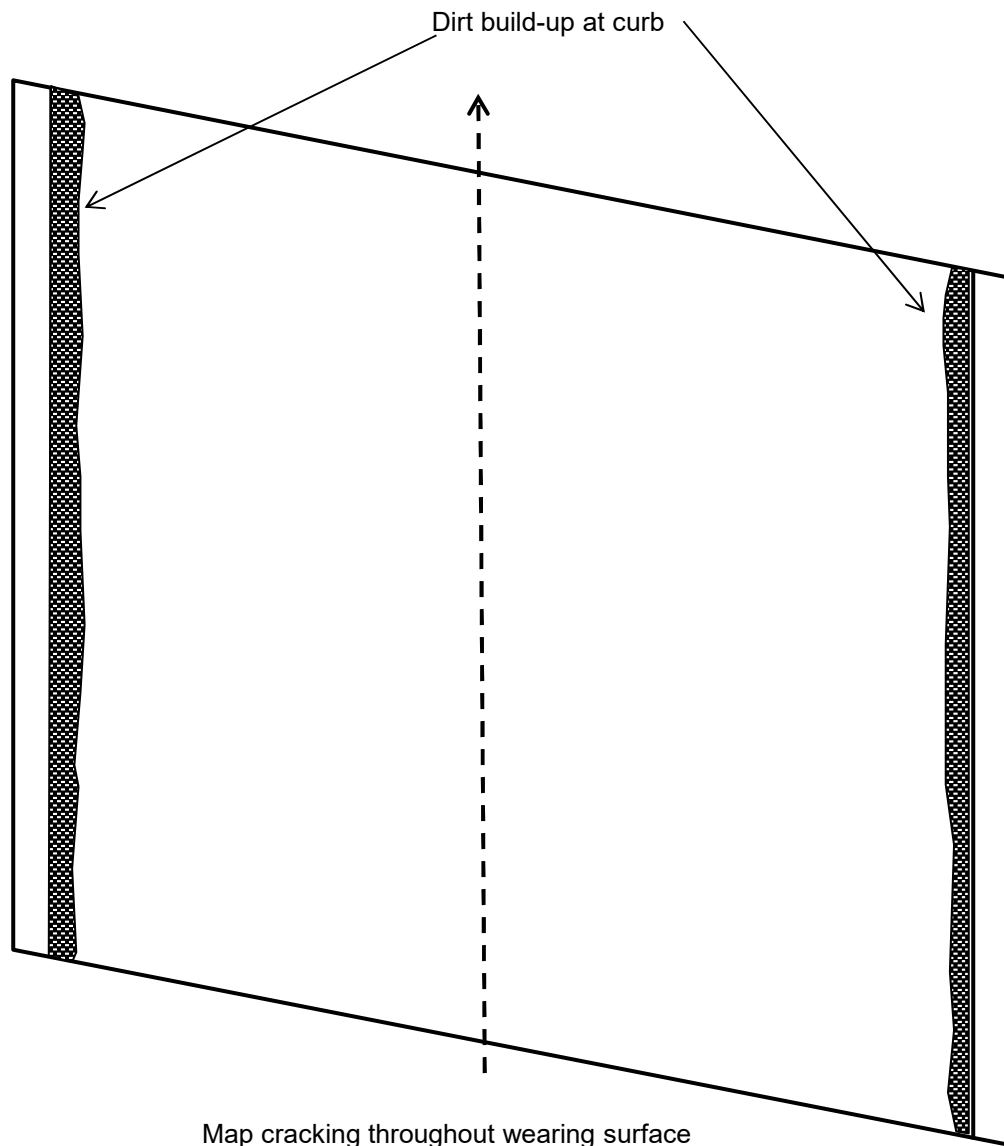


Top of Deck Span No. 2

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking		Repairs		(light vertical)
	Spalling		Voids		(large checker board)
	Scaling		Dimensions are noted as		
	Delamination				
		(confetti)			
		(10% dots)			
		(40% gray)			
			← X ↑ X depth		

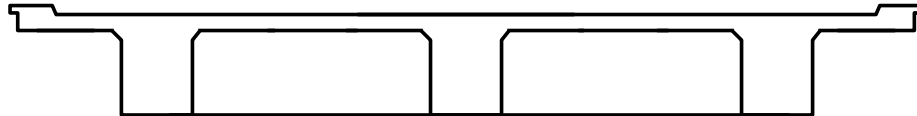


Bottom of Deck Span No. 2

7/17/ 07/13/22

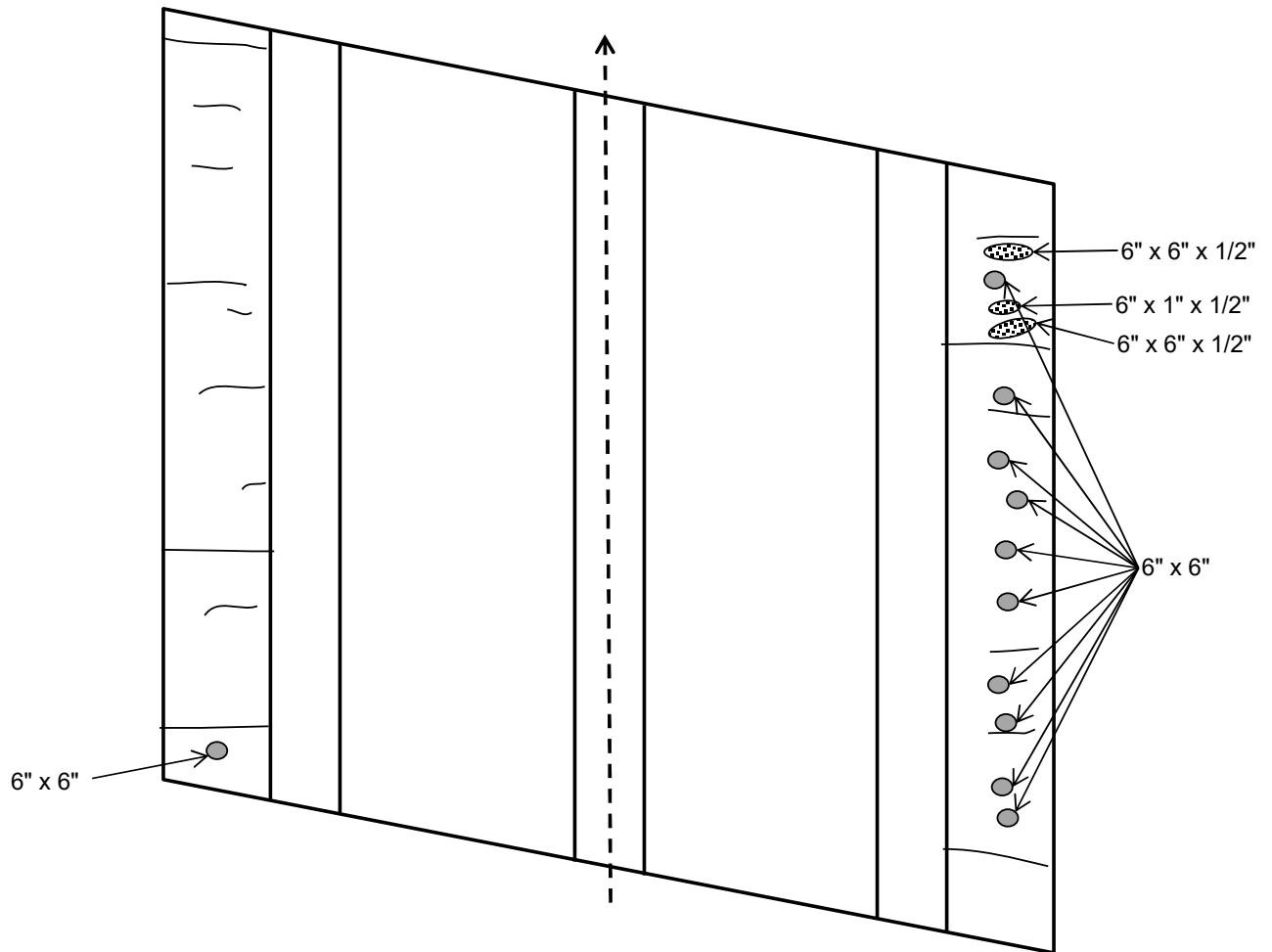
Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Voids		(large checker board)
	Scaling		(10% dots)	Dimensions are noted as		
	Delamination		(40% gray)			
				← X ↑ X depth		



-Cracks are hairline with efflorescence

-All spalls have rebar

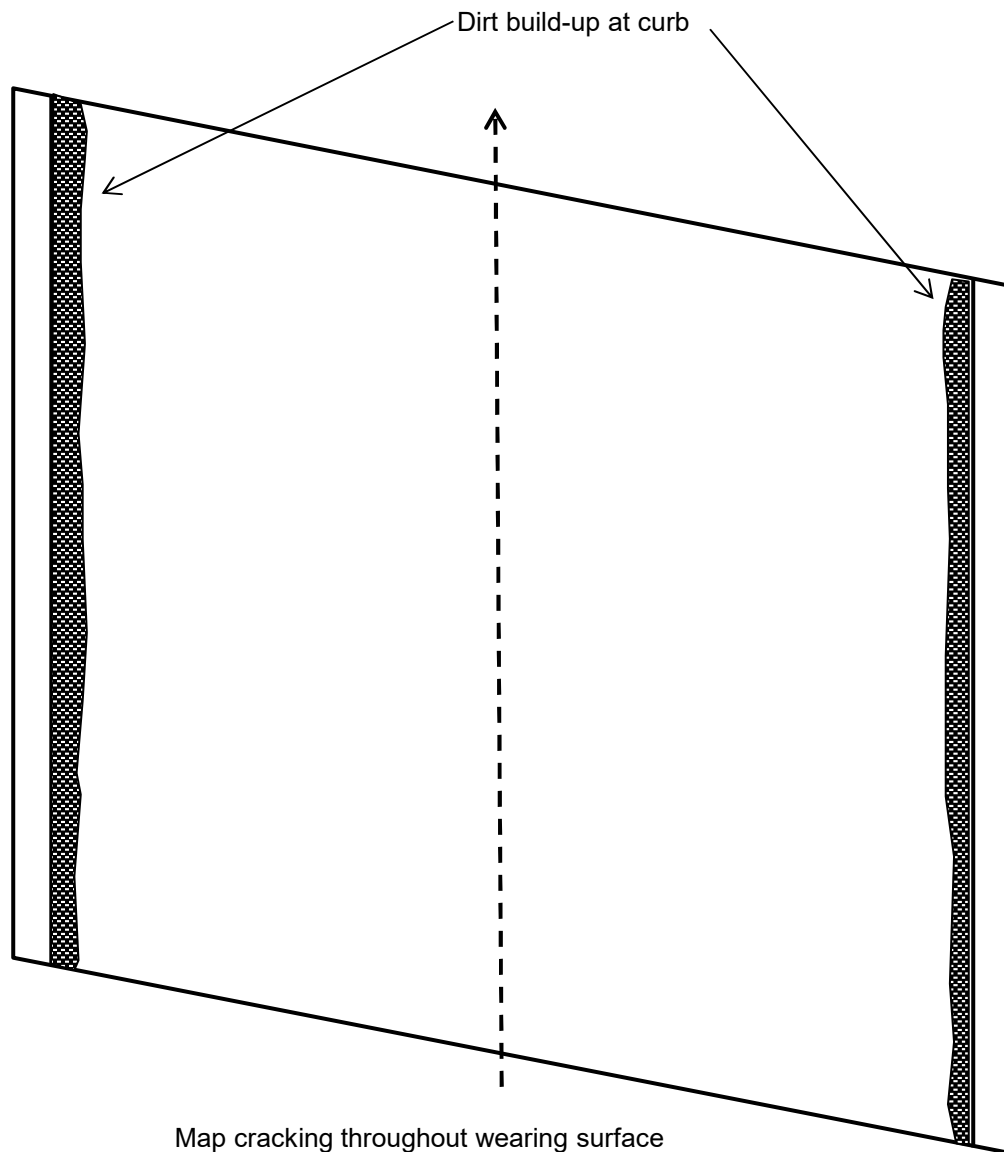


Top of Deck Span No. 3

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking		Repairs		(light vertical)
	Spalling		Voids		(large checker board)
	Scaling		Dimensions are noted as		
	Delamination				
		(confetti)			
		(10% dots)			
		(40% gray)			
			← X ↑ X depth		



Map cracking throughout wearing surface

Bottom of Deck Span No. 3

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:

Cracking



Repairs



(light vertical)

Spalling



(confetti)

Voids



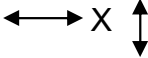
(large checker board)

Scaling



(10% dots)

Dimensions are noted as

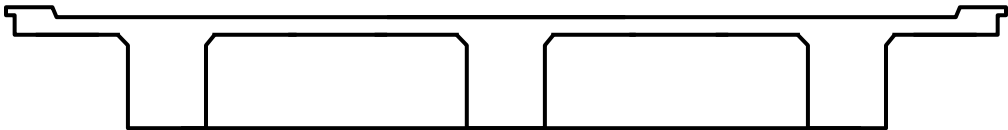


X X depth

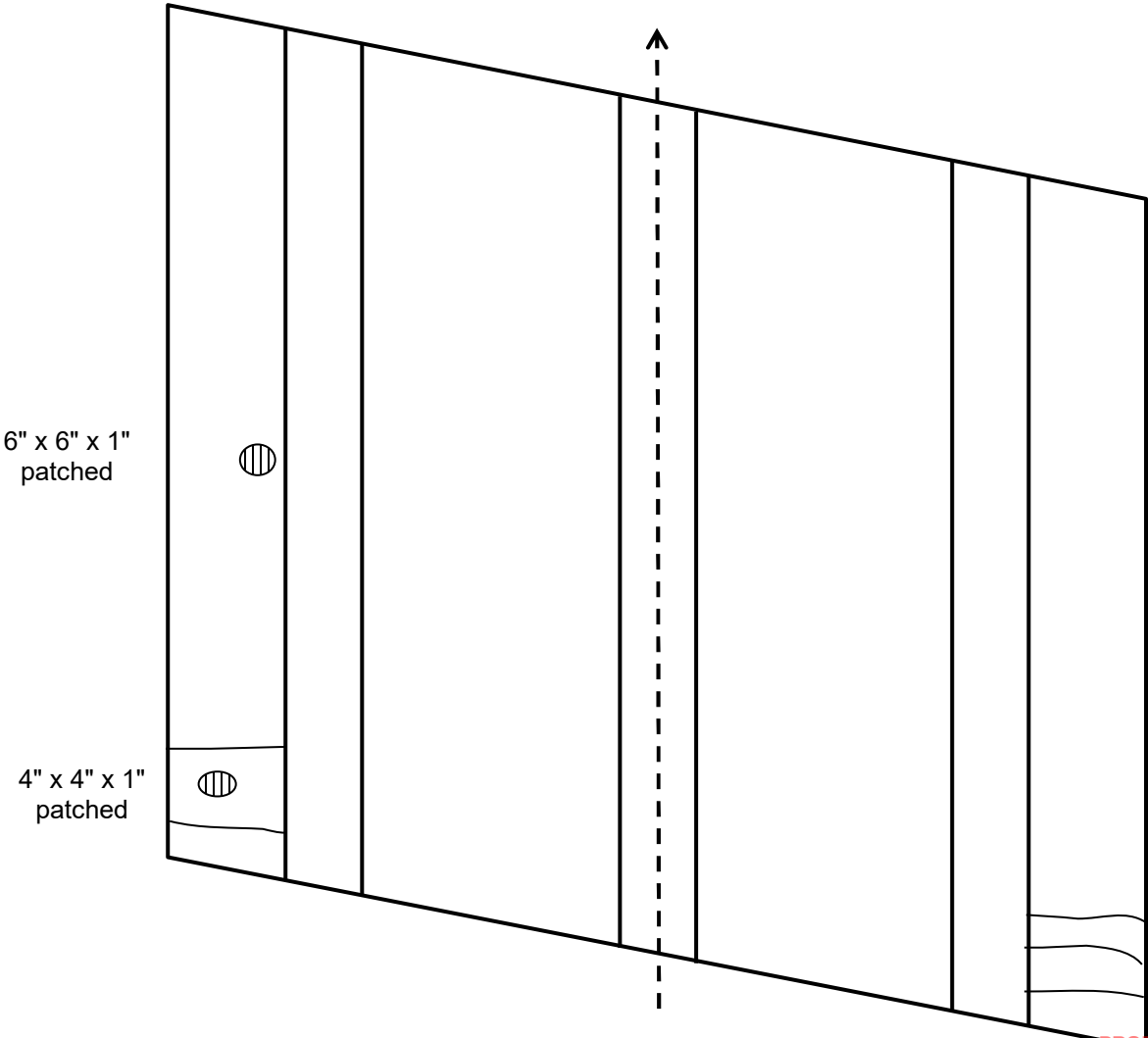
Delamination



(40% gray)



cracks are hairline with efflorescence



Beam Details - Span No. 3

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Voids		(large checker board)
	Scaling		(10% dots)	Dimensions are noted as		
	Delamination		(40% gray)	X X depth		

Beam "A" - Right Side



Beam "A" - Left Side



Beam "B" - Right Side



Beam "B" - Left Side



Beam Details - Span No. 3

Date 07/17/24

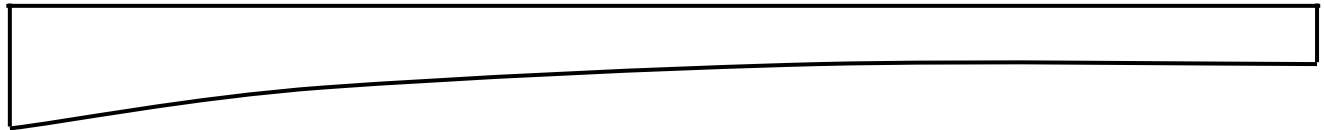
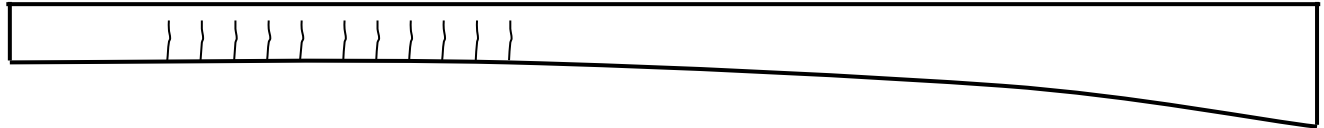
Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
				Voids		(large checker board)
	Spalling		(confetti)	Dimensions are noted as		
	Scaling		(10% dots)	X X depth		
	Delamination		(40% gray)			

Beam "C" - Right Side



Beam "C" - Left Side

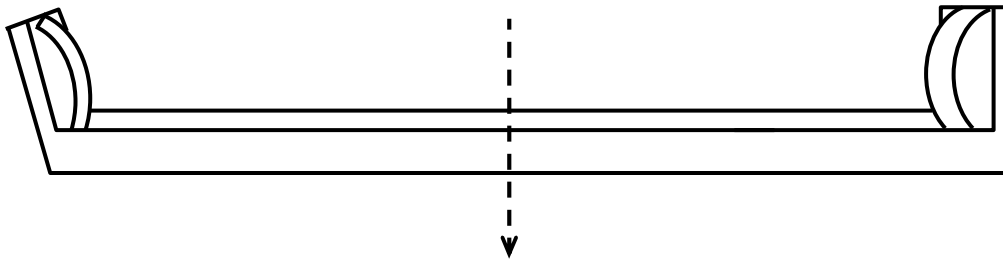


Abutment No. 1

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Bearings		(light upward diagonal)
	Scaling		(10% dots)	Voids		(large checker board)
	Delamination		(40% gray)	Dimensions are noted as		
				 X  X depth		

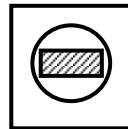
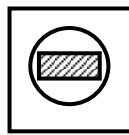
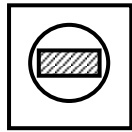


Bent No. 1

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Bearings		(light upward diagonal)
	Scaling		(10% dots)	Voids		(large checker board)
	Delamination		(40% gray)	Dimensions are noted as  X  X depth		

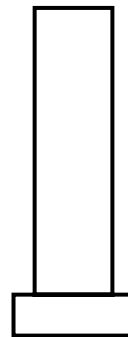
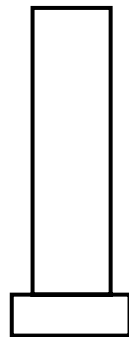
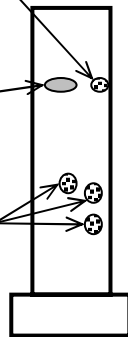


Front Side

3" x 3" x 1/2"
with rebar

8" x 4"

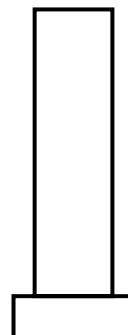
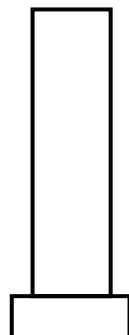
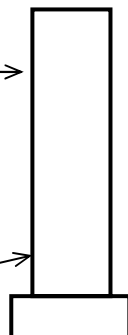
5" x 5" x 1/2"
with rebar



Back Side

1" x 1" x 1/2"
with rebar

4" x 4" x 1/2"
with rebar

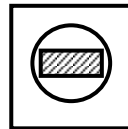
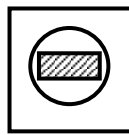
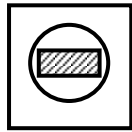


Bent No. 2

Date 07/17/24

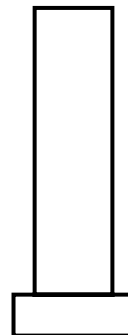
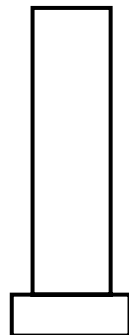
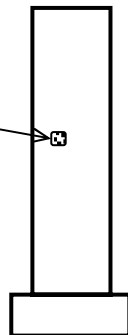
Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Bearings		(light upward diagonal)
	Scaling		(10% dots)	Voids		(large checker board)
	Delamination		(40% gray)	Dimensions are noted as		
					X	 X depth

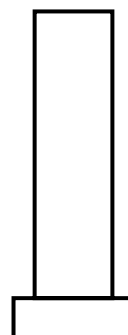
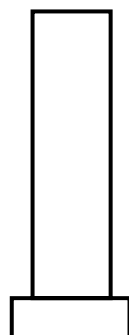
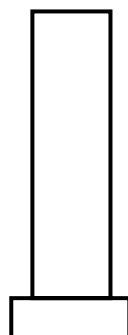


Front Side

3" x 3" x 1/2"



Back Side

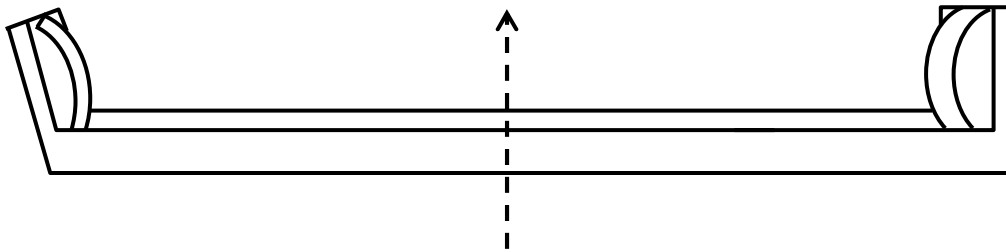


Abutment No. 2

Date 07/17/24

Bridge Location No. 58 02161 1.36
County Route Log Mile

Legend:	Cracking			Repairs		(light vertical)
	Spalling		(confetti)	Bearings		(light upward diagonal)
	Scaling		(10% dots)	Voids		(large checker board)
	Delamination		(40% gray)	Dimensions are noted as		
				 X  X depth		



Form BIR 3.10

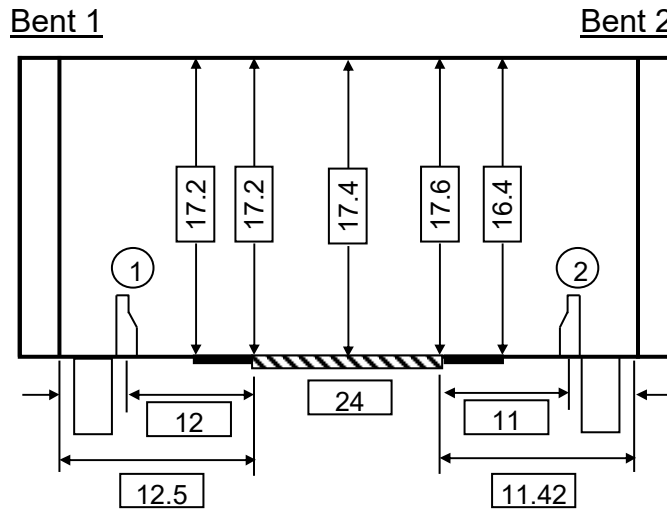
REVISED 6-9-92

Date 07/17/24

Bridge Location No. 58 2161 1.36
County Route Log Mile

NOTE: ALL DISTANCES AND ELEVATIONS ARE IN FEET.

Lateral and Vertical Clearances for One Lane Highway



1. Rail / Barrier Type:

W-Shape

☐

Conc. Barrier

☒

None

☐

2. Rail / Barrier Type:

W-Shape

☐

Conc. Barrier

☒

None

☐

Equipment List

General Inspection

☒ Yes Pocket knife
☒ Yes Sounding/chipping hammer
☐ Chain drag
☒ Yes Range pole
☐ 25' rod - depth and clearance

Visual Aid

☐ Binoculars
☐ Flashlight
☐ Magnifying glass
☐ Hand mirror

Cleaning

☐ Wisk broom
☐ Wire brush
☐ Flat bladed screwdriver
☐ Hand shovel
☐ Penetrating oil (WD-40, etc.)

Tools For Access

☐ Ladders
☐ Rope
☐ Waders
☐ Machete or bush axe

Comment

Reach-All Approval and Comments

Tools For Measuring

☐ Masonry/Wood Ruler
☒ Yes 6' Pocket Tape
☐ 25' and 100' Tape
☐ Calipers
☐ Thermometer
☐ Carpenter's Level
☐ String and Weighted line (plumb bob)

Special Purpose Equipment

☐ Reach All
☐ Bucket Truck
☐ Traffic control
☐ Boat
☐ Sonar depth finder
☐ Increment borer
☐ Survey equipment
☐ Safety Harness
☐ Climbing equipment
☐ Dye penetrant
☐ Drone
☐ Air Meter

Special Purpose Equipment

ELEMENTS	DESCRIPTION	UNITS	TOTAL	CS1	CS2	CS3	CS4
16	Re Conc Top Flange	SF	4185	4103	78	4	0
1080	Delamination/Spall/Patched Area	SF	22	0	18	4	0
1120	Efflorescence/Rust Staining	SF	60	0	60	0	0
510	Wearing Surfaces	SF	3906	2203	0	1703	0
3220	Crack (Wearing Surface)	SF	1703	0	0	1703	0
(16) Element record added 2016-07-25.							
(1080-16) Element record added 7/23/2020							
(1120-16) Element record added 7/20/2022							
(510-16) Element record added 2016-07-25.							
(3220-510-16) Element record added 7/20/2022							
110	Re Conc Opn Girder/Beam	LF	419	419	0	0	0
(110) Element record added 2016-07-25.							
205	Re Conc Column	EA	6	3	3	0	0
1080	Delamination/Spall/Patched Area	EA	1	0	1	0	0
1090	Exposed Rebar	EA	2	0	2	0	0
(205) Element record added 2016-07-25.							
(1090-205) Element record added 7/20/2022							
215	Re Conc Abutment	LF	42	42	0	0	0
(215) Element record added 2016-07-25.							
310	Elastomeric Bearing	EA	6	6	0	0	0
(310) Element record added 2016-07-25.							
330	Metal Bridge Railing	LF	279	279	0	0	0
(330) Element record added 2016-07-25.							